

Welcome to the Plainfield Road Corridor Study Public Information Meeting #1. This presentation will be reviewed during the live virtual meeting on April 14th and a pre-recorded version is on the project website: ImprovePlainfieldRoad.org. Please see the website for registration to the virtual meeting.

If you have questions, comments, or feedback on this project for the project team please see the end of the presentation and project website for ways to provide comments during the official comment period.

Goals for Today

- Introduce the Project and Project Team
- Provide an Overview of the Project Goals and Activities
- Review the Public Outreach Strategy and Activities
- Review Existing Condition Along the Corridor
- Present the Proposed Alternatives for Consideration
- Identify Next Steps

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The slide features a blue header with the title 'Goals for Today'. Below the title, six teal-colored horizontal bars list the goals, each accompanied by a small icon. The background on the left side of the slide shows a photograph of a paved road with a yellow center line, viewed from a low angle. A large blue curved shape overlaps the left side of the slide, partially covering the road image.

This presentation is about 30 minutes long. Within the presentation we will:

- Introduce you to the project and Project Team
- Provide an overview the project goals and activities
- Review the public outreach strategy and activities
- Review existing conditions along the corridor
- Present the proposed alternatives for consideration
- Identify next steps

Project Team



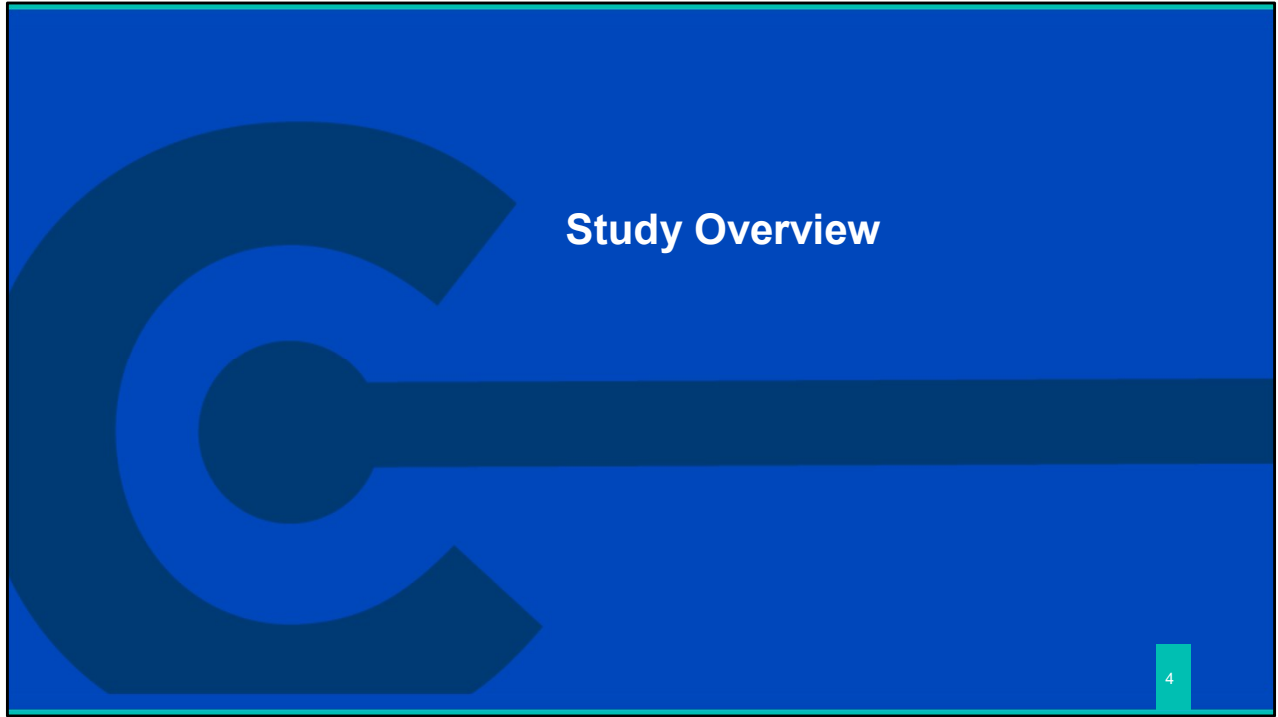
KNIGHT

Engineers & Architects

METRO
strategies

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This project is led by the Cook County Department of Transportation and Highways (CCDoTH) and its consultant team, including Knight Engineers and Architects and Metro Strategies.



First, we will provide a study overview.

Study Area

The study corridor is approximately 3.5 miles long, from County Line Road to East Avenue.



- Village of Burr Ridge
- City of Countryside
- Village of Indian Head Park
- Village of La Grange
- Village of McCook
- Village of Western Springs
- La Grange Highlands / Lyons Township

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The study area extends along Plainfield Road from County Line Road to East Avenue. This stretch of roadway is approximately 3.5 miles long. The corridor traverses through the Village of Burr Ridge, Village of Indian Head Park, Village of Western Springs, City of Countryside, La Grange Highlands/Lyons Township, Village of La Grange, and Village of McCook.

Cook County has jurisdiction over Plainfield Road within the study area. This means that the County is responsible for routine maintenance, repairs or reconstruction of the road.

Study Background

The existing roadway pavement is reaching the end of its 60+ years of service life. Reconstruction of the roadway is anticipated in order to provide optimal service to its users and address known community concerns along the Plainfield Road corridor.

This Phase I study will accomplish the following:

- Review the existing conditions on the corridor
- Coordinate with adjacent on-going projects and studies
- Solicit stakeholder and public input to develop the study goals and guide the study process
- Develop and assess corridor improvement alternatives
- Provide a preferred alternative to advance to Phase II (design) engineering and construction



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Plainfield Road is surrounded by a variety of land uses and the existing roadway pavement is reaching the end of its 60+ years of service life. We anticipate reconstruction of the roadway to provide optimal service to the corridor users and address the known concerns.

This study will review the existing conditions, coordinate with adjacent on-going and future projects and studies, request community and public feedback and input to help guide the study process, develop and assess corridor improvement alternatives, and provide a preferred alternative.

Study Goals



Safety improvements for all users



Enhanced connectivity and accommodations for bicyclists and pedestrians



Reconstruct and update roadway and drainage infrastructure



Accommodate projected year 2050 travel demands



*Brainard Avenue cycle track

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The goals of the study include:

- Improving safety for all users
- Enhancing connectivity and accommodations for bicyclists and pedestrians
- Reconstructing and making updates to the roadway and drainage infrastructure
- And accommodating the projected year 2050 travel demands and improving operational efficiency along the corridor

Typical Project Timeline

- The project team is currently in Phase I: Preliminary Engineering and Environmental Study
- Phase II and Phase III are not yet funded



The Public Involvement Plan is available on the project website: ImprovePlainfieldRoad.org

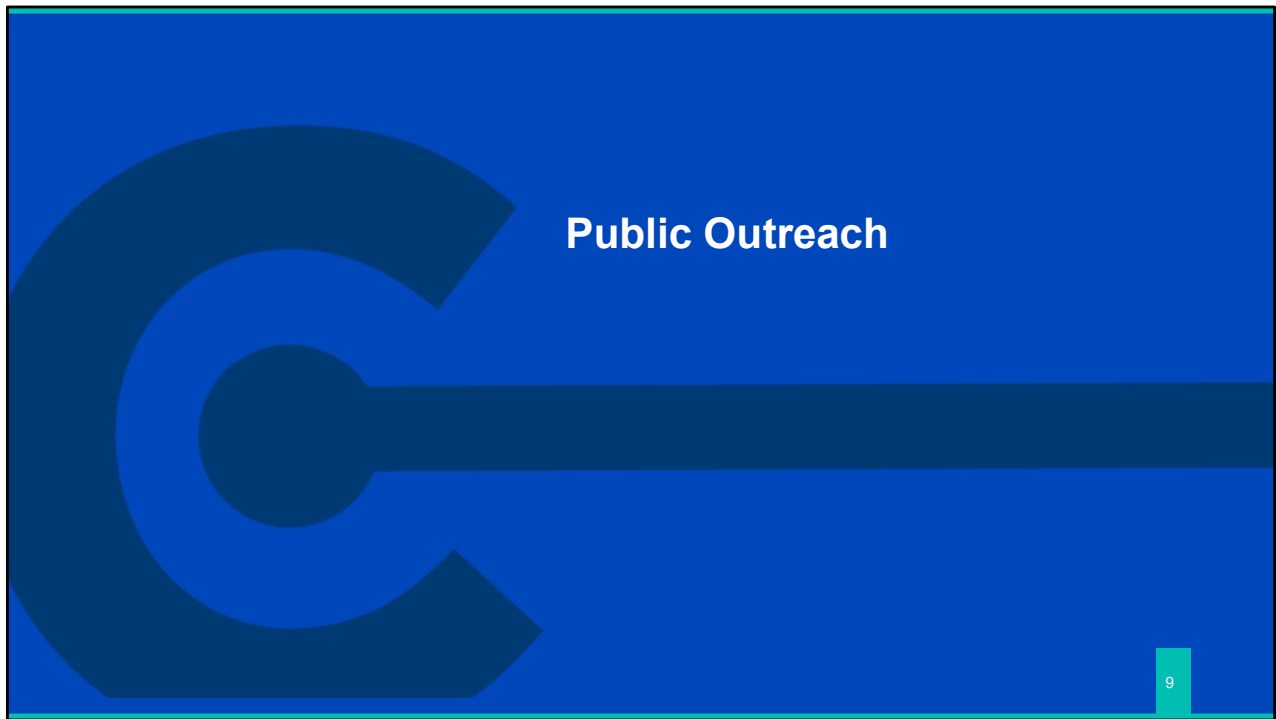
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The graphic on this slide shows the typical project development process. As you can see, we are in Phase I: Preliminary Engineering and Environmental Study, which is expected to last approximately 24-36 months.

- Next is Phase II: Contract Plan Preparation and Land Acquisition, which is expected to last approximately 18-24 months and
- Lastly, Phase III, or the Construction phase, which is expected to last approximately 18-24 months.

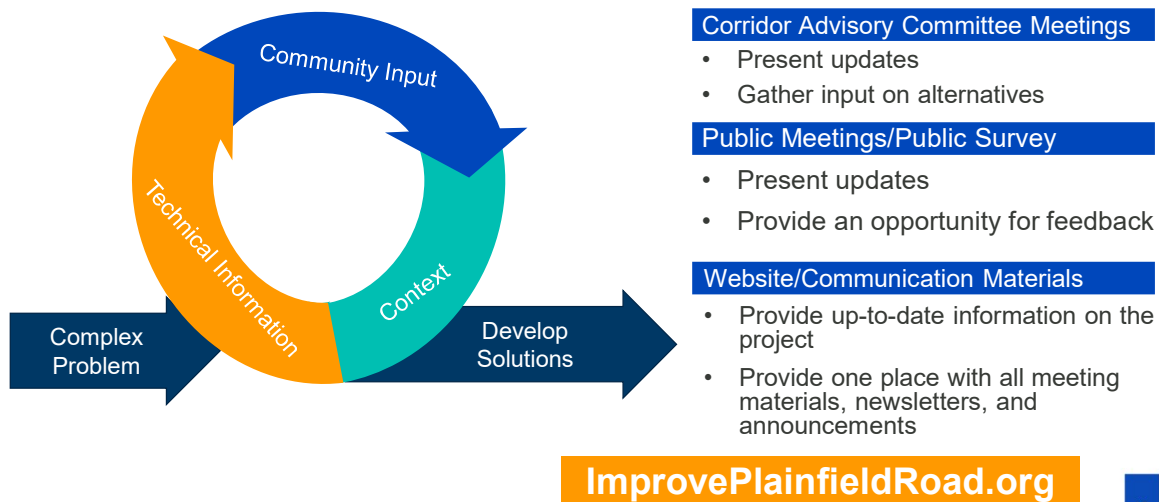
It is important to note that Phase II and III are not yet funded, and the Project Team will investigate future funding opportunities as Phase I progresses.

It is also important to highlight that public engagement will occur during each phase of the project. The Project Team has developed a robust Public Involvement Plan for Phase I, which outlines the public involvement strategy for the project. This plan is available on the project website: Improveplainfieldroad.org.



The next few slides will outline key components of that public involvement plan and summarize our outreach efforts completed to date.

Context Sensitive Solutions Process



This project will follow the context sensitive solutions process as outlined by the Federal Highway Administration and Illinois Department of Transportation. This process is an interdisciplinary approach that seeks effective, multimodal transportation solutions by working with community members to develop, build and maintain cost-effective transportation facilities which fit into and reflect the project’s surroundings or its “context”. We will attempt to do this through early, frequent, and meaningful communication with community members, and a flexible and creative approach to design. The resulting project should improve safety and mobility for the traveling public, while seeking to preserve and enhance the scenic, economic, historic, and natural qualities of the surrounding corridor.

In order to meet these core principles, our Project Team has developed three main ways to collaborate with the local communities.

The first opportunity is the Corridor Advisory Committee. This Committee is comprised of local municipality and agency representatives, as well as representatives from various community organizations located along the corridor. The purpose of this committee is to help provide additional information and feedback to the Project Team at key points throughout the project.

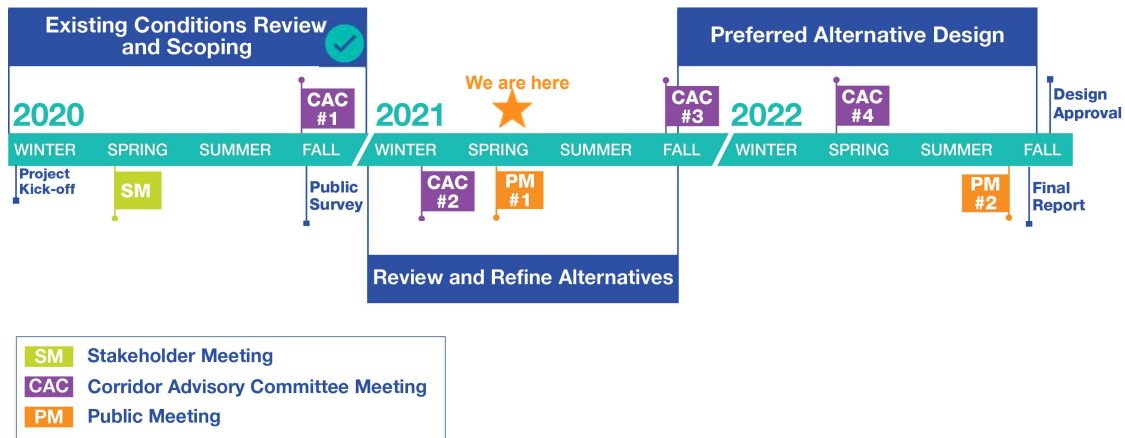
The next collaboration opportunity is to gather a wider range of public and community input

via public meetings and a public survey. This public information meeting is just one initiative in this category. We are planning a second public information meeting later in the project and have already completed a public survey on the existing conditions. We will share those results in a few slides.

The final opportunity for us to collaborate with the local communities is by providing communication materials and updates on the project website: ImprovePlainfieldRoad.org. This website will be used throughout the project for public information meeting materials as well as overall study updates. Please check out the website, share it, and let us know if you have any ideas to improve it.

The public involvement process is critically important to ensure the project meets the needs and desires of the local community as well as keeps the project eligible for future state and federal funding.

Phase I Study Timeline



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The graphic shown is a timeline of the Phase I activities described earlier.

As represented on the graphic, the study began in early 2020 and we are projecting it will be completed by the fall of 2022.

We recently completed the existing conditions review and project scoping phase, which we will present in a few slides. The existing conditions review and project scoping included and considered input received during our Municipal Coordination Meetings, our Corridor Advisory Committee Meetings, and the public survey.

We are beginning our second year of the study and the Project Team is now reviewing and refining alternatives.

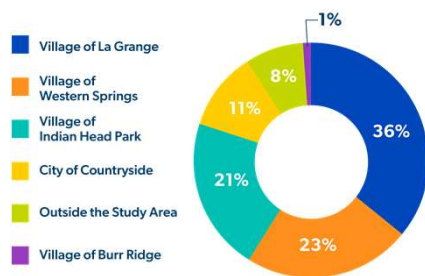
After a preferred alternative is selected, the team will advance the preliminary design of the selected alternative and obtain all required reviews and approvals from the Illinois Department of Transportation, Federal Highway Administration, and other regulatory agencies.

Throughout the process the Project Team will be seeking input from various community members such as yourselves.

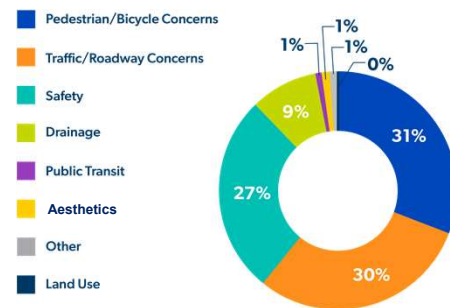
Public Survey 2020 Highlights

- Responses were accepted September 8 – September 29
- **230 participants** completed the welcome questionnaire
- **162 comments** were provided on the interactive map

Respondents' Hometown



Respondents' Priorities



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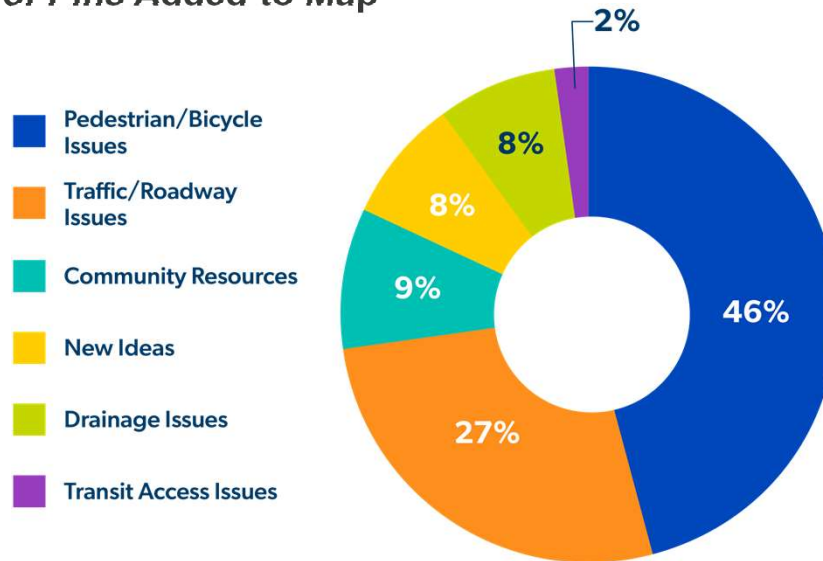
As previously discussed, the Project Team conducted a public survey on existing conditions in September of 2020, these next few slides will summarize the comments and input. The survey included a welcome questionnaire and an interactive map to receive input from the community on issues they see or are experiencing, key locations or areas of interest the Project Team should know about, and finally provide an opportunity for community members to share their ideas and thoughts to improve Plainfield Road.

A total 230 participants completed the welcome questionnaire. The welcome questionnaire asked basic demographic questions as well as what respondents thought should be the top priority for improving Plainfield Road. From the welcome questionnaire, the greatest number of respondents live in the Village of La Grange, followed by Western Springs and Indian Head Park.

When identifying what participants thought should be the priority focus area in the corridor, ped/bike and traffic/roadway concerns each received about 30%, followed by general safety with 27%.

Participants were also able to input pins and comments on pins on an interactive map. 162 comments were left on the interactive map.

Types of Pins Added to Map

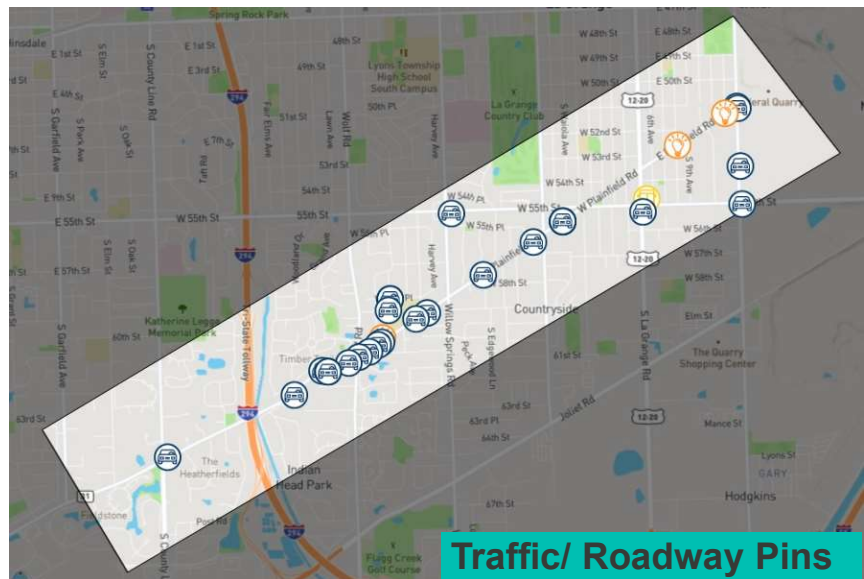


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Looking further into the types of pin that were added to the interactive map, pedestrian/bicycle issues were nearly 50% of the total pins, followed by traffic/roadway at 27%. This is really interesting because the comments on the survey both complement the CAC meetings, but also expand and highlight the community's focus on the importance of safe pedestrian and bicyclist facilities along this corridor.

Public Survey Highlights

- Many comments were received regarding traffic concerns near Wolf Road, specifically areas where it is hard to turn on or off Plainfield Road safely
- Comments near 55th Street discussed areas of congestion and asked for turn lanes

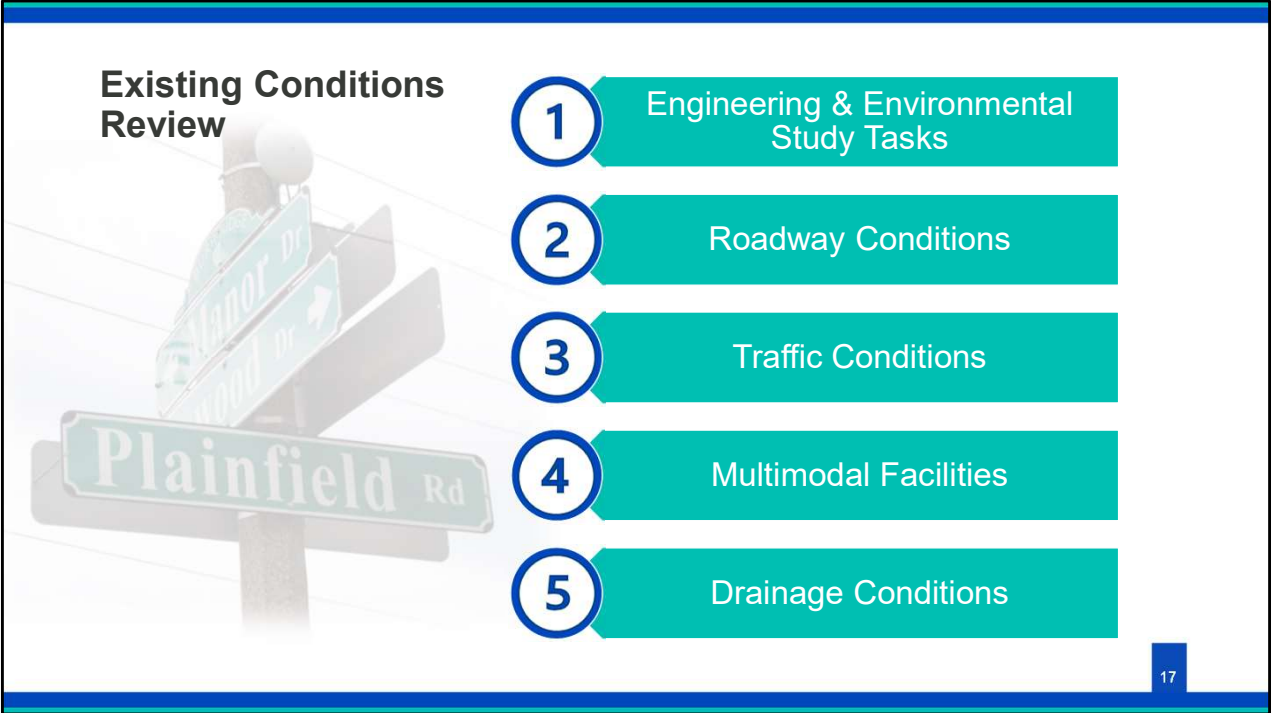


The traffic/roadway concerns or issues identified heavily focused on areas along the corridor where it is hard or unsafe to make turning movements onto and off of Plainfield Road. As you can see the majority of these comments are grouped near Wolf Road. There were also multiple comments on the pins placed near 55th Street all discussing congestion in that area and asking for turn lanes.



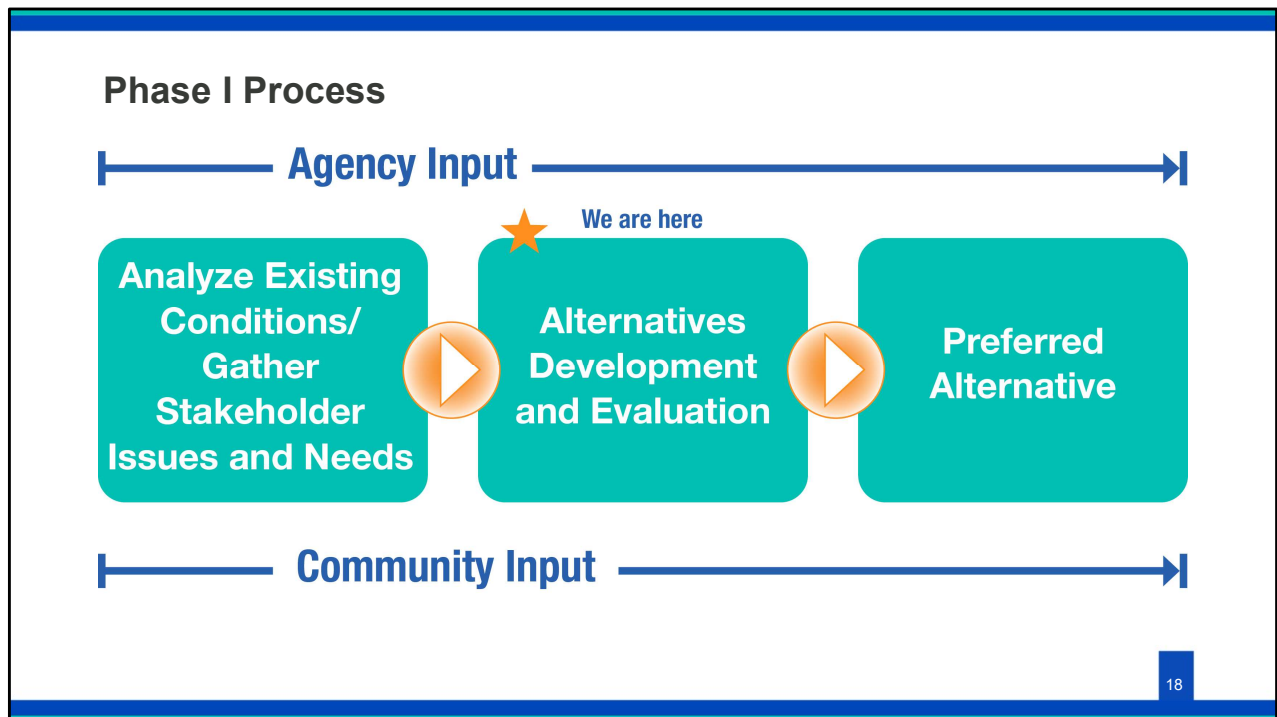
Now that you have an understanding of what the project goals are, the general timeline, and the planned and completed public outreach; we are going to review existing conditions and summarize data collection activities performed to date. The data we've collected has led us to several proposed alternatives for the corridor, which we will discuss later during this presentation.

Existing Conditions Review



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During this section of the presentation, we will provide an overview of the engineering and environmental study tasks being undertaken as part of this Phase I study. We will also discuss the following four components of existing conditions along the corridor; roadway conditions, traffic conditions, multimodal facilities, and drainage conditions.



As mentioned previously, we are currently in Phase I of the project.

The Phase I process involves analyzing the existing conditions to evaluate deficiencies and gather input on community issues and needs.

This input, along with the existing conditions analysis, is used to develop the proposed improvement alternatives which we will discuss with you today. The proposed improvement alternatives are then evaluated based on a variety of factors, and a single preferred alternative is recommended.

Environmental Studies and NEPA Process



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During the Phase I Study, the Project Team performs a variety of environmental studies that assess how the proposed improvements may impact noise, wetlands, cultural and historic resources, and recreational spaces such as parks and Forest Preserves.

The environmental studies process includes:

- Creating an inventory of resources
- Evaluating how the alternatives could impact these resources
- Identifying ways to avoid and minimize impacts where possible
- And mitigating the unavoidable impacts from the proposed improvements

Engineering and Environmental Study Tasks



Tasks Completed/ In Progress

Topographic Survey, Review of Existing Plans and Studies, Crash Analysis, Traffic Studies, Existing Drainage Studies, Wetland Delineation, Special Waste Assessment (PESA), Biological and Cultural Resource Review, Geotechnical Studies, Pavement Analysis


Tasks Not Yet Started

Selection of Preferred Alternative, Preliminary Roadway and drainage Design for the Preferred Alternative, Right-Of-Way Impact Evaluation, Parking Analysis, Noise Studies (if applicable)

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As part of the Phase I process, the Project Team has completed or is in the process of completing: a topographic survey, a review of existing plans and studies, crash analysis, traffic projections and analysis, existing drainage analysis, environmental reviews, including wetlands, special waste, and biological and cultural resources, as well as geotechnical studies, and pavement analysis. These tasks completed to date have provided the Project Team with enough information to develop the proposed alternatives we will present to you today.

It is important to note that while we have accomplished a lot over the past year, there are still several key tasks that have not yet begun and will need to be completed after we receive your feedback on our preliminary alternatives. After we receive this feedback, the Project Team will select a preferred alternative and develop the preliminary roadway and drainage design for this alternative. While we currently have a general understanding of the impacts that could be associated with each alternative, advancing the preliminary design for the preferred alternative will provide more detailed information on the amount of Right-Of-Way (ROW) the project will require, as well as how parking will be impacted along the corridor. Depending on the preferred alternative selected, a noise analysis may also be required.



Please note that while some sections of the corridor have had recent resurfacings the subgrade and subbase are original and needs repair

Pavement Conditions

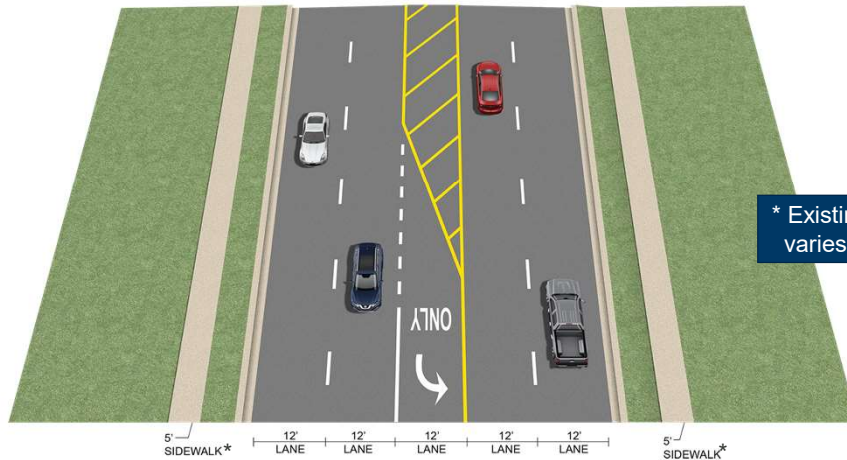
- All pavement sections of asphalt and concrete have reached the end of their useful service life.
- Throughout the project corridor, various sections have been widened and resurfaced over the years, however the subbase and subgrade of the existing pavement are all original.
- Additional considerations for proposed improvements including adjacent projects, timeline, and keeping roads open to traffic will be made.

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Throughout the corridor the pavement type and age vary, but all pavement sections have reached the end of their useful service life and require improvements. Near County Line Road the concrete pavement is more than 40 years old and in other areas along the corridor the asphalt pavement is more than 60 years old.

It is important to note that, while there are portions of the corridor that have been widened and resurfaced over the years, the subbase and subgrade of the pavement is original. It is anticipated that the pavement will likely be reconstructed throughout the entire project limits. Looking ahead to planning construction, the Project Team will take into consideration adjacent projects, such as the Tollway's replacement of the Plainfield Road bridge over I-294 and coordinate closely with those agencies on project timelines and construction staging plans. We will develop a construction staging plan that attempts to keep the roads open to traffic.

Existing Typical Cross Section – County Line Road to Shady Lane



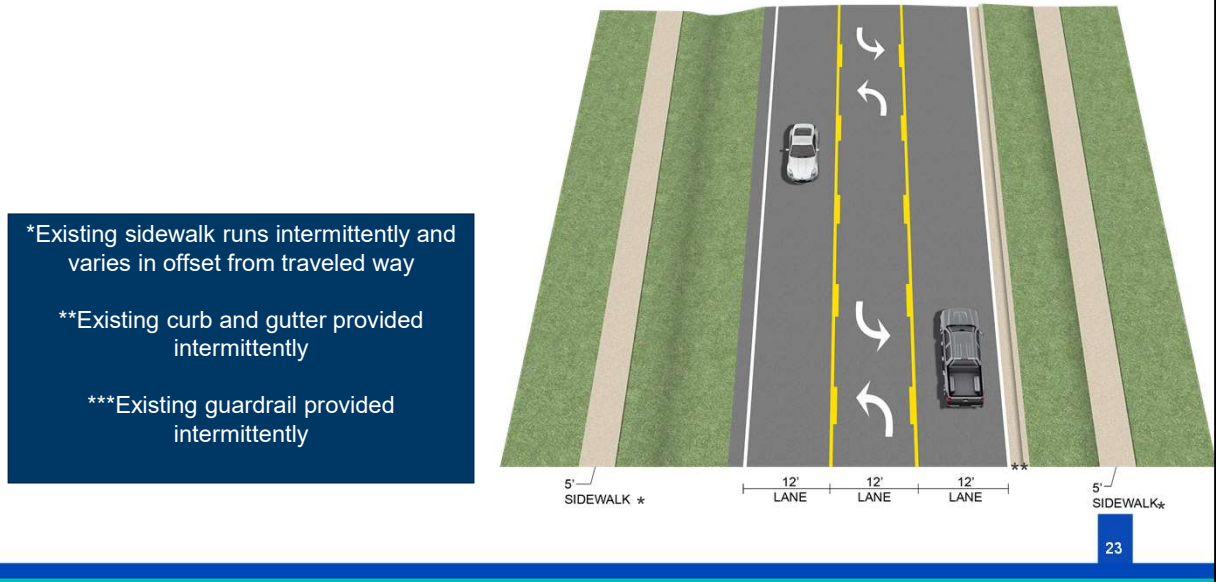
* Existing sidewalk runs intermittently and varies in its offset from the back of curb

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The following three slides outline the existing typical cross sections throughout the corridor.

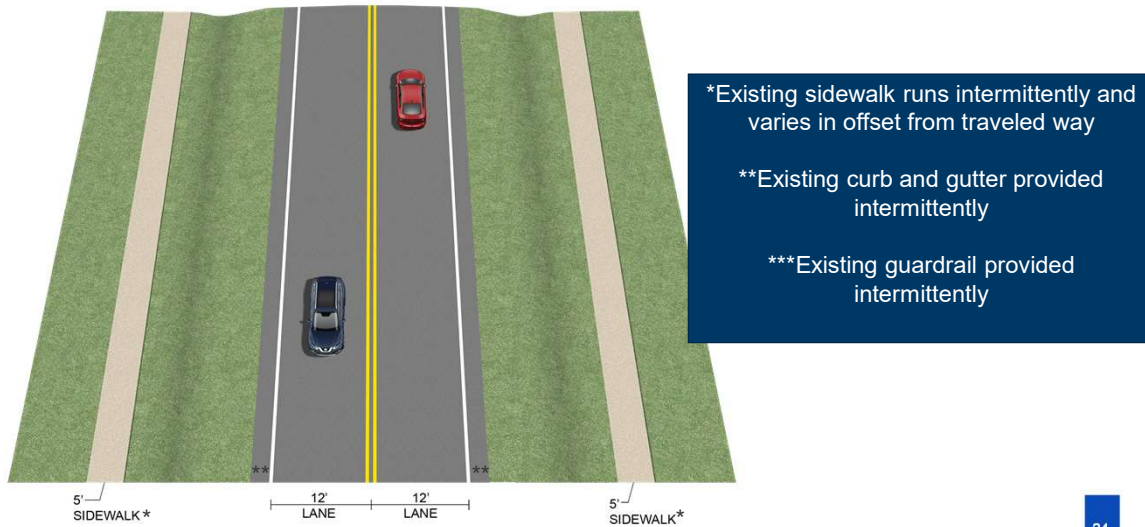
The section between County Line Road and Shady Lane consists of two 12-foot lanes in each direction, a 12-foot center left turning lane and curb and gutter. There are intermittent 5-foot sidewalk sections with a varying distance from the roadway. Gaps in sidewalk sections throughout the corridor will be highlighted later in the presentation.

Existing Typical Cross Section – Shady Lane to Willow Springs Road



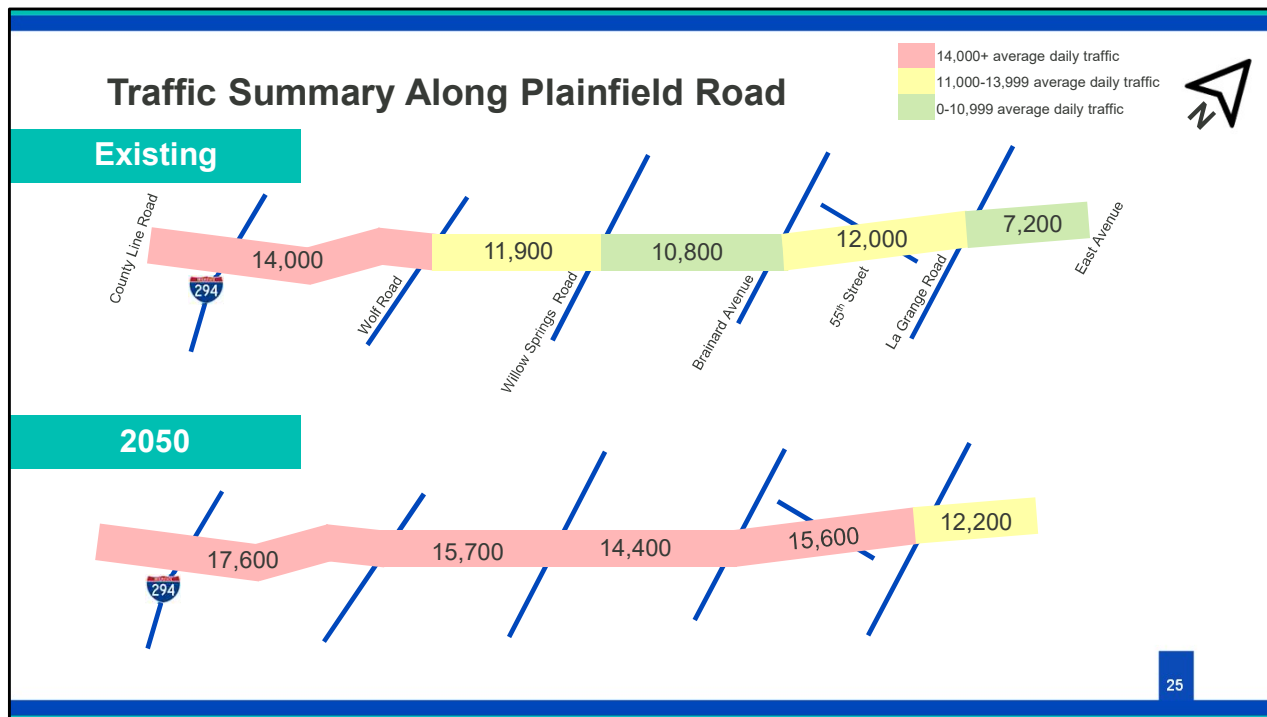
The section between Shady Lane and Willow Springs Road consists of one 12-foot lane in each direction and a 12-foot center left turning lane. There are 5-foot sidewalks present at select segments which vary in their offset from the roadway. Guardrail and curb and gutter are present intermittently throughout this section of the corridor.

Existing Typical Cross Section – Willow Springs Road to East Avenue

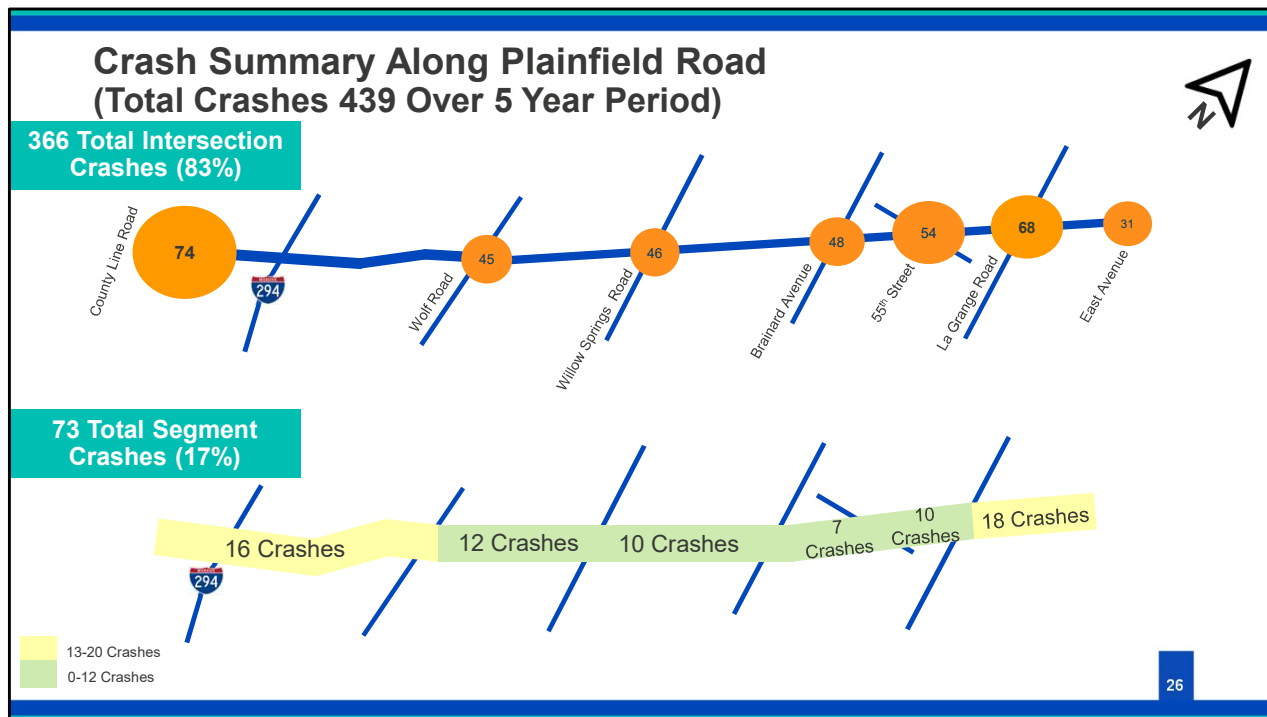


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The section between Willow Springs Road to East Avenue represents the majority of the corridor and consists of one 12-foot lane in each direction with no median and includes an open ditch drainage system. Additionally, this section has intermittent sidewalks and guardrails.



Traffic Data Collection was completed along Plainfield Road, which is classified as a major collector route. It is important to highlight that the traffic counts were collected in March 2020, prior to the Illinois “Stay at Home Order” due to the COVID-19 pandemic. The following shows the existing and projected traffic conditions. The green segments represent locations within the project limits along Plainfield Road with lower counted Average Daily Traffic (ADT) and red segments represent locations with the higher counted Average Daily Traffic. From the graphic, you can see that the existing traffic between County Line Road and Wolf Road is the highest within the corridor and the future 2050 projections from Chicago Metropolitan Agency for Planning (CMAP) outline an increase in traffic. The graphic shows that the average daily traffic is anticipated to increase throughout the corridor.



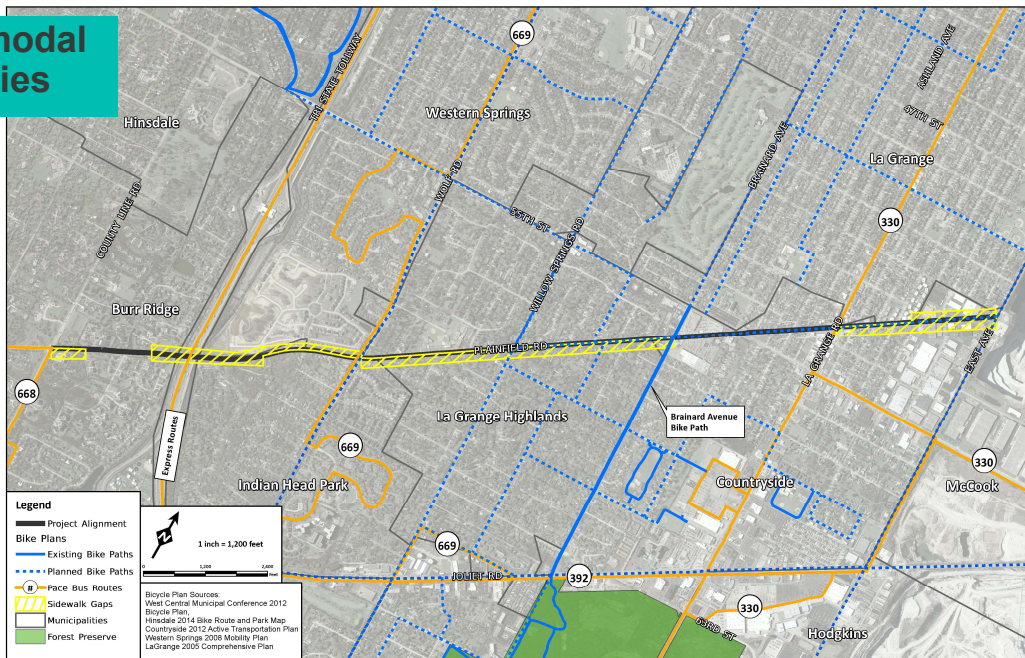
The following slide summarizes the crash data obtained by the Project Team. There were a total of 439 crashes along the corridor over the 5-year period analyzed. Of the total crashes along the corridor there were no fatalities and roughly 18% of crashes were non-visible injuries.

Of the total crashes, 42% were rear end crashes.

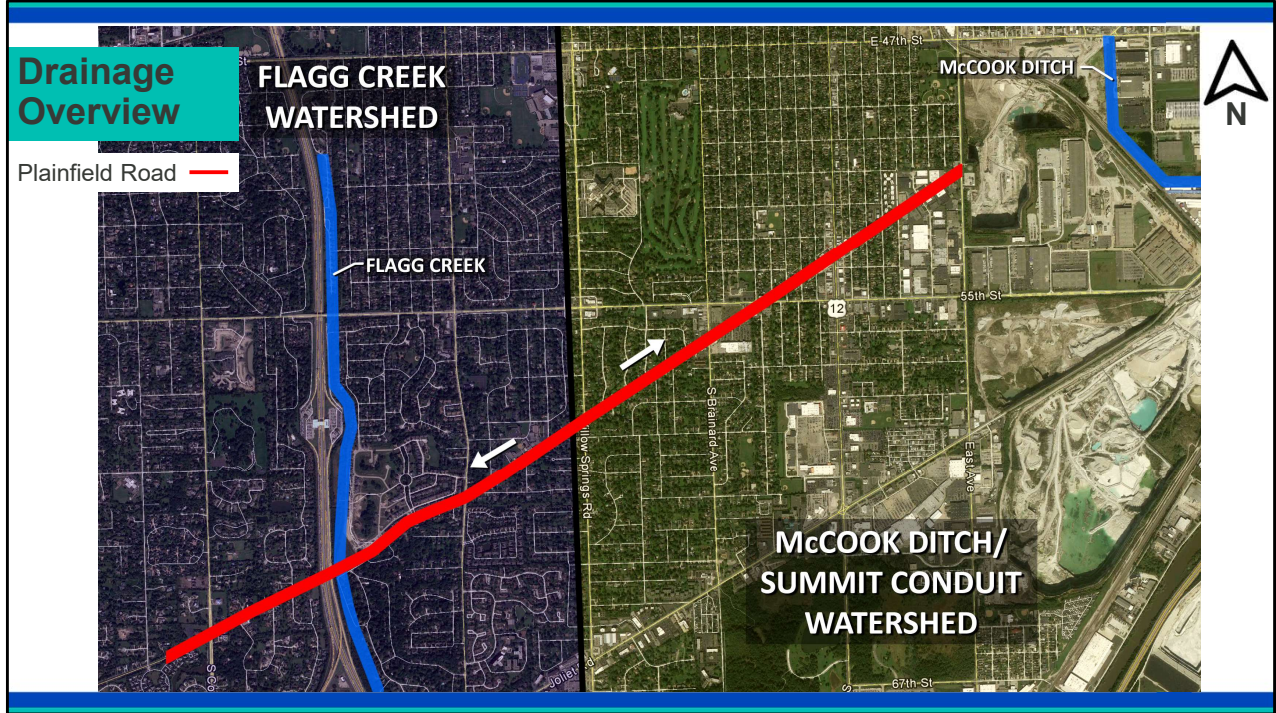
As you can see, the majority of crashes at signalized intersections have occurred at the intersections of Plainfield Road and County Line Road and Plainfield Road and La Grange Road.

Of the segments between intersections, the highest number of crashes occurred between La Grange Road and East Avenue.

Multimodal Facilities



This image shows the multimodal facilities which are currently present or planned throughout the corridor. As you can see there are a variety of existing and planned bike paths in blue and dashed blue, as well as four Pace routes crossing or near the corridor in orange. You can also see a very distinct gap in continuous sidewalk along the corridor in hatched yellow.



There are two watersheds servicing the corridor, one is the Flag Creek watershed and the other is the McCook Ditch/Summit Conduit watershed. In general, within the corridor limits, water west of Willow Springs Road drains to Flag Creek and water east of Willow Springs Road drains to the McCook Ditch/Summit Conduit watershed. The Project Team is aware of the drainage concerns throughout the corridor and is considering those in the development of the alternatives for consideration.

A detailed drainage design will not be developed until a preferred alternative is selected; however, the proposed drainage plan is anticipated to provide stormwater detention in accordance with the MWRD Watershed Management Ordinance permit requirements and aims to not make conditions worse than what is there today.

The Project Team is also coordinating with the ongoing MWRD Drainage Studies within the corridor.



Now, we will take a look at how we developed the potential alternatives and the resulting alternatives for consideration.

Building Toward the Design Alternatives



Existing Conditions Data

- Environmental Data Collection
- Traffic and Crash Analysis
- CAC #1 Input
- Public Survey Data



Purpose and Need



Design Alternatives

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This slide shows the process which was used by the Project Team for the development of the various alternatives.

The Existing Conditions Data Collection Phase was completed and included the various engineering and environmental studies noted previously, Corridor Advisory Committee input, and Public Survey input.

A Draft Purpose and Need statement was developed in coordination with IDOT and the Federal Highway Administration.

All of the review, analysis and community input was used to develop the various alternatives which will be presented today.

Evaluation Criteria

Identified Needs

- Safety
- Mobility and Operational Efficiency
- Pedestrian/Bicycle Accommodations and Connectivity
- Upgrade Roadway and Drainage Infrastructure

Potential Property Impacts

- Residential
- Commercial
- Recreational

Cost

- Construction
- Maintenance

Potential Environmental Impacts

- Parks
- Trees
- Wetlands

Accessibility

- Pedestrians
- Bicycles

Community Input

- Key Stakeholders
- General Public

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Listed here are the preliminary evaluation criteria for refining and finalizing a preferred alternative. The Project Team will consider safety, mobility and access, and pedestrian and bicyclist accommodations in conformance with the Americans with Disabilities Acts (ADA) requirements. In addition, roadway and drainage needs, costs, and impacts to both properties and environmental resources within the study area will be considered. Your input will also be factored into the decision-making process.

Design Alternatives Considerations

Design alternatives presented today are high level concepts based on corridor, traffic and community needs. These concepts provide the project team with enough information to understand the general impacts associated with each alternative.

The prepared alternatives utilize standard dimensions from IDOT and CCDoTH design criteria to represent typical offsets, lane widths, sidewalk widths, and shared use path widths.

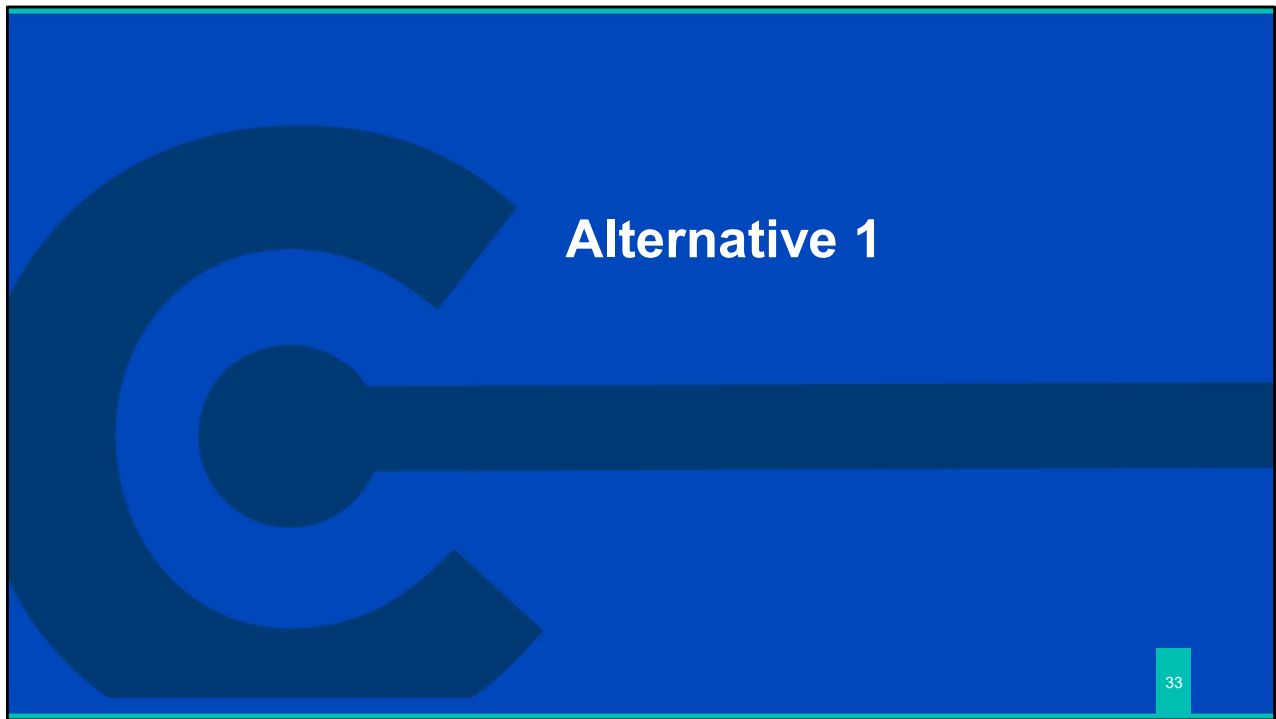
Additional design details and impacts will be evaluated during the next stage of the study, after public feedback on the alternatives is received.

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Before we present the proposed alternatives, it is important to note that the alternatives we will show you today are high level concepts based on corridor, traffic and community needs. These concepts provide the Project Team with enough information to understand the general impacts associated with each alternative.

In general, the prepared alternatives utilize standard dimensions from IDOT and CCDoTH design criteria to represent typical offsets, lane widths, sidewalk widths, and shared use path widths.

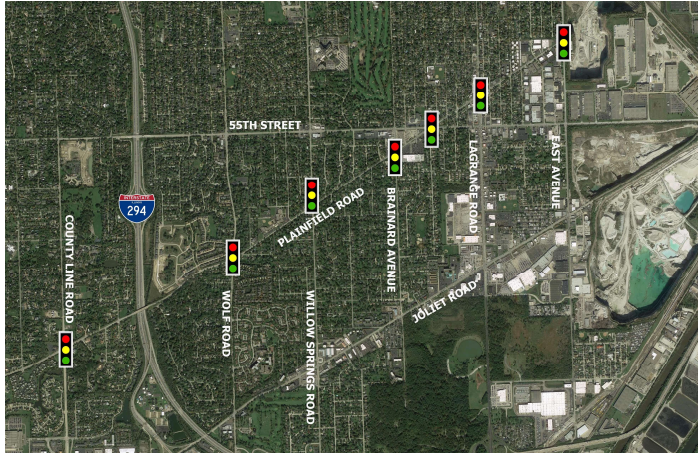
Additional design details and impacts, including right-of-way and parking impacts, environmental impacts, as well as exact locations of the roadway, sidewalks, and shared use paths, will be evaluated during the next stage of the study, after public feedback on the alternatives is received.



There were four alternatives that were developed and investigated based on data collection and input received from the Corridor Advisory Committee and Public Survey.

Alternative 1

Proposed Intersection Improvements



RECOMMENDED IMPROVEMENT TYPES

- Signal optimization
- Added right turn lanes
- Added left turn lanes
- Design vehicle upgrades

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Alternative 1 includes various improvements to the signalized intersections along the corridor where traffic analysis has identified existing and future capacity deficiencies with poor traffic operations. The recommended improvements increase operational efficiency where poor levels of service are anticipated or have currently been identified. The intersection improvements would also accommodate larger design vehicles to allow for the safer travel of trucks.

This alternative does not include any corridor-wide changes in lane configurations (such as a continuous center left turn lane) and does not add a sidewalk or shared use path.

Alternative 1 Pros and Cons

Pros	Cons
<ul style="list-style-type: none">• Improves Level of Service at each intersection• Minimal Right of Way impacts for Intersection improvements	<ul style="list-style-type: none">• No continuous accommodation for pedestrians and bicycles• No opportunities for drainage improvements• No additional segment mobility for motorists• No opportunities for segment safety improvements• Minor impacts to section 4(f) property at Arrowhead Memorial Point Park at the southeast corner of Wolf Road and Plainfield Road will be impacted by intersection improvements and shared use path

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After an initial review, Alternative 1 is not recommended as a standalone alternative based on the Pros and Cons shown

The Project Team has highlighted the strengths of Alternative 1 as improving the overall level of service at intersections and resulting in minimal Right of Way impacts at each intersection.

The Project Team also noted that Alternative 1 did not provide continuous accommodations for pedestrians and bicycles, provides minimal opportunities for drainage improvements, and does not provide additional improvements in mobility and safety along the segments between the intersections.

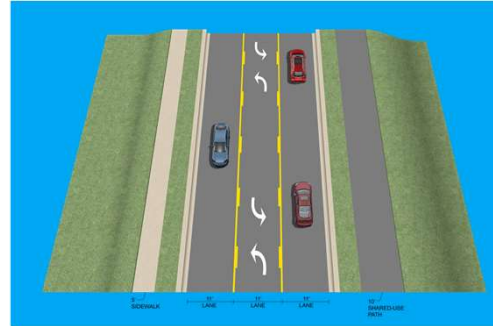
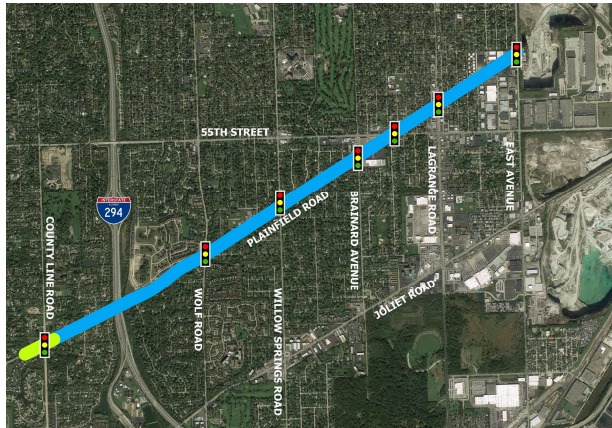
Additionally, the intersection improvements would impact a section 4(f) property, Arrowhead Memorial Point Park at the intersection of Wolf Road and Plainfield Road.

Because Alternative 1 does not provide a continuous shared use path or sidewalk, opportunities for drainage improvements, or segment mobility and safety improvements, it is not recommended for further investigation as a standalone alternative, as previously mentioned. These intersection improvements, however, are included as part of other alternatives under consideration.



Alternative 2 includes the recommended intersection improvements in Alternative 1 and reconstructs Plainfield Road between these signalized intersections.

Alternative 2 Typical Section



- Intersection Improvements
- 3-lane roadway
- Continuous sidewalk
- Continuous shared use path
- Curb and gutter

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The recommended typical section for Alternative 2 is a 3-lane roadway, which includes one travel lane in each direction with a continuous center left turn lane. A sidewalk and a shared use path are also proposed throughout the corridor. Under this alternative, the existing 5-lane roadway near County Line Road would remain.

Additionally, a closed drainage system, including curb and gutter, is anticipated throughout the corridor.

Additional design details, including sidewalk and shared use path offsets, will be considered after the preferred alternative is selected to minimize impacts to adjacent properties, sensitive areas, trees, parking, drainage, and other considerations

Alternative 2 Aerial Exhibit



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We will now walk through an aerial view of Alternative 2. Note that the shared use path is shown on the south side of Plainfield Road in red and the sidewalk is shown on the north side of Plainfield Road in blue. The existing ROW is shown as a dotted in red line.

Beginning at the intersection at County Line Road, improvements include the addition of northbound dual left turn lanes, a southbound right turn lane, an eastbound right turn lane and a raised median on the northbound and southbound approach. Additionally, traffic signal timing optimization will be investigated for this intersection and all other signalized intersections within the corridor. The intersection improvements will also accommodate larger design vehicles to allow for the safer travel of trucks.

The existing 5-lane section on Plainfield Road would remain and then taper to a 3-lane section northeast of Shady Lane, as it does in the existing condition.

The 3-lane typical section and continuous shared use path and sidewalk would then be carried to the I-294 Bridge.

The I-294 Bridge will be replaced under a separate project as a part of the Tollway improvements to I-294. The bridge will accommodate the required through lanes, a shared use path on the south and a sidewalk on the north.

Alternative 2 reflects the coordination between Cook County and the Tollway to provide the much-needed connections of the sidewalk and shared use path over the Tollway bridge.

The 3-section would continue north of the I-294 Bridge providing the center two way left turn lane, a continuous shared use path, and a continuous sidewalk.

Approaching the Wolf Road intersection, improvements include the addition of eastbound and westbound right turn lanes.

The 3-lane section would continue past the LaGrange Highlands School Campus with the sidewalk remaining on the north and the shared use path remaining on the south sides of Plainfield Road.

It is important to note that the offset of the sidewalk and shared use paths from the back of curb are preliminary throughout the corridor and attempts will be made to minimize impacts in the next stage of study.

The existing lane configuration at the intersection of Willow Springs Road would remain.

The three-lane section with a two way left turn lane, a sidewalk on the north and a shared use path on the south sides of Plainfield Road would continue east.

It is also important to note that Alternative 2 intends to provide a curb and gutter with a closed drainage system for use throughout the entire corridor along Plainfield Road.

Approaching the intersection at Brainard Ave, improvements would include the addition of northbound and southbound right turn lanes.

The lane configuration at the intersection of 55th Street is not suggested to change; however, the existing storage lengths for the northbound right turn lane and eastbound and westbound left turn lanes would be extended.

The three-lane section, sidewalk and shared use path is continuous north of 55th Street as we approach the intersection of LaGrange Road.

No changes to the existing lane configuration are recommended at the intersection of LaGrange Road, however the existing eastbound left turn lane storage length would be increased.

A 3-lane section would continue for the remainder of the study area to East Avenue.

It is possible that the shared use path would terminate at 8th Avenue. A sidewalk would then continue, along with the 3-lane section, on the north and south sides of Plainfield Road to East Ave.

The intersection at East Avenue would provide dual left turn lanes and a right turn lane from Plainfield Road to East Avenue. The lane configuration along East Ave would remain the same.

Alternative 2 Pros and Cons

Pros	Cons
<ul style="list-style-type: none">• Improves Level of Service throughout corridor• Provides consistent accommodations for pedestrians and bicycles• Provides opportunities for drainage improvements• Provides additional segment mobility for motorists• Improves safety by providing a refuge for left turning vehicles• Minimal Right of Way impacts along the corridor	<ul style="list-style-type: none">• Minor impacts to section 4(f) property at Arrowhead Memorial Point Park at the southeast corner of Wolf Road and Plainfield Road by intersection improvements and shared use path• Existing parking impacts• Shrubbery and tree removal required

39

The Project Team conducted an initial review of Alternative 2 and noted the Pros and Cons shown.

Alternative 2 improves the level of service throughout the corridor, provides continuous accommodations for pedestrians and bicycles, provide opportunities for drainage improvements, and improves safety and mobility at both the intersections and segments between the intersections. Alternative 2 also results in minimal anticipated Right of Way impacts along the corridor.

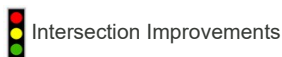
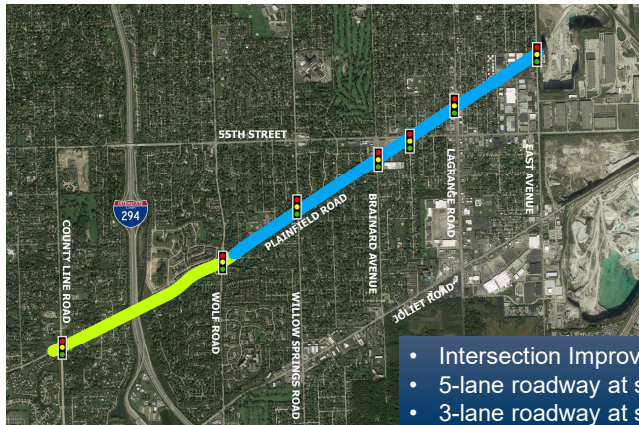
Alternative 2 would result in section 4(f) impacts to Arrowhead Memorial Point Park, require shrubbery and tree removal, and result in impacts to existing parking adjacent to the corridor.

Alternative 2 is recommend by the Project Team for further investigation.



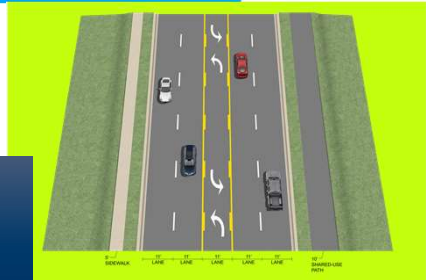
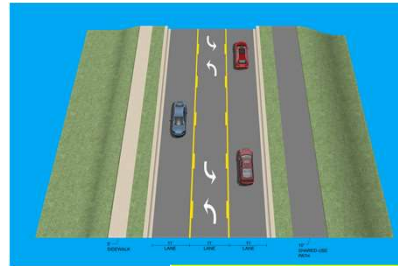
The next alternative that was developed was Alternative 3. This alternative includes all of the improvements contained in Alternatives 1 and 2 but introduces a wider 5-lane roadway within select segments of the Plainfield Road Corridor. There were two segments along Plainfield Road in which the traffic data dictated that a 5-lane roadway should be analyzed according to IDOT design criteria. The two segments were separated in two alternatives, Alternative 3A and Alternative 3B. It is noted that both segments could stand alone or be combined and result in Alternative 3C.

Alternative 3A Typical Section



Intersection Improvements

- Intersection Improvements
- 5-lane roadway at select locations
- 3-lane roadway at select locations
- Continuous sidewalk
- Continuous shared use path
- Curb and gutter



41

The first segment in which a 5-lane roadway was analyzed was between County Line Road and Wolf Road. The typical section through this segment would provide two through lanes in each direction and a center left turn lane. The remainder of the corridor would consist of a 3-lane roadway as depicted in Alternative 2. A shared use path and sidewalk would be proposed continuously along the corridor.

Alternative 3A would also introduce a closed drainage system for use along the entire corridor by providing curb and gutter.

Alternative 3A Aerial Exhibit



42

An aerial exhibit has been prepared for Alternative 3A. Only the segment between County Line Road and Wolf Road will be included in the Alternative 3A walkthrough. All other segments of the corridor not shown in the Alternative 3A dynamic walkthrough are proposed to be improved as shown in Alternative 2. The aerial exhibits representing Alternative 3A can be viewed on the project website. Note that the shared use path is shown on the south side of Plainfield Road in red and the sidewalk is shown on the north side of Plainfield Road in blue. The existing ROW is shown as red a dotted red line.

The intersection at County Line Road would, again, provide dual northbound left turn lanes, a southbound right turn lane, an eastbound right turn lane and a raised median on County Line Road

The 5-lane section along Plainfield Road would then continue through to Garywood Drive which would include a two way left turn lane, curb and gutter with an enclosed drainage system, a shared use path on the south side and sidewalk on the north side of Plainfield Road.

Plainfield Road would then taper to a 4-lane section to cross over the I-294 Bridge,

which, as previously mentioned, will be replaced as a part of the Tollway improvements to I-294.

Alternative 3A would also carry the much-needed connection of shared use path and sidewalk over the I-294 bridge.

The 5-lane section would then resume from west of Keokuk Road and include a two way left turn lane, curb and gutter, a shared use path on the south side of Plainfield Road and a sidewalk on the north side of Plainfield Road.

The 5-lane section would continue through the Wolf Road intersection where improvements would include the addition of eastbound and westbound right turn lanes.

Plainfield Road would then taper to a the 3-lane section northeast of Wolf Road for the remainder of the corridor limits as described in Alternative 2 with the shared use path on the south side of Plainfield Road and the sidewalk on the north side of Plainfield Road.

Alternative 3A Pros and Cons

Pros	Cons
<ul style="list-style-type: none"> • Improves Level of Service throughout corridor • Provides consistent accommodations for pedestrians and bicycles • Provides opportunities for drainage improvements • Provides additional segment mobility for motorists • Improves safety by providing a refuge for left turning vehicles • Provides additional capacity between County Line Road and Wolf Road 	<ul style="list-style-type: none"> • Additional Right of Way impacts to provide additional lane • No significant operational benefits vs. anticipated impacts • Municipal opposition to 5-lane section through this segment • Minor Impacts to section 4(f) property at Arrowhead Memorial Point Park at the southeast corner of Wolf Road and Plainfield Road will be impacted by intersection improvements and shared use path • Shrubbery and tree removal required • Impacts to existing parking

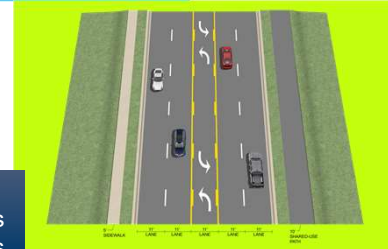
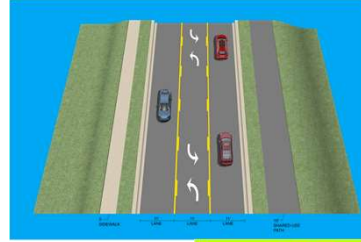
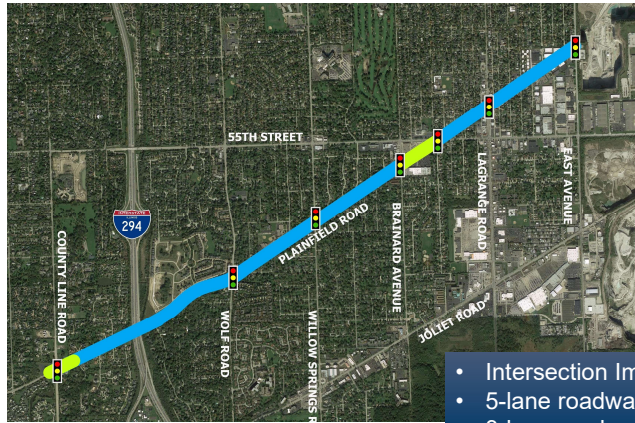
43

The Project Team noted the pros and cons in the initial review of Alternative 3A summarized in the graphic. Consistent with the other alternatives, Alternative 3A improves the level of service at intersections throughout the corridor, provides continuous accommodations for pedestrians and bicycles, provides opportunities for drainage improvements, and provides additional segment operational efficiency and safety. The additional through lane in each direction provides additional capacity for motorists between County Line Road and Wolf Road.

The Project Team has already received opposition from the local municipalities to a 5-lane roadway within this segment of the corridor. The 5-lane section is anticipated to cause additional ROW Impacts without providing significant additional operational benefits. The increase in impervious area would cause larger stormwater impacts that need to be addressed. Alternative 3A would result in larger section 4(f) impacts to Arrowhead Memorial Park at the intersection of Wolf Road and Plainfield Road than Alternatives 1 and 2 due to the added through lanes.

Alternative 3A is recommended for further investigation. IDOT design requirements and projected traffic data between County Line Road and Wolf Road supports the need to continue analyzing a 5-lane section in this area.

Alternative 3B Typical Section



- Intersection Improvements
- 5-lane roadway at select locations
- 3-lane roadway at select locations
- Continuous sidewalk
- Continuous shared use path
- Curb and gutter

 Intersection Improvements

44

The next segment that a 5-lane roadway was analyzed is located between Brainard Avenue and 55th Street and referenced as Alternative 3B.

The proposed typical section in this segment is similar to Alternative 2 but includes a continuous eastbound and westbound right turn lane, to provide safer access into the various commercial driveways located in this section of the corridor. A 3-lane roadway would be provided for the remainder of the corridor. The shared use path and sidewalk would be proposed continuously throughout the corridor.

As shown in all the previous alternatives, curb and gutter would be provided for use along the corridor.

Alternative 3B Aerial Exhibit



45

An aerial exhibit has been prepared for Alternative 3B. Only the segment between Brainard Avenue and 55th Street will be included in the Alternative 3B dynamic walkthrough. All other segments of the corridor not shown in the Alternative 3B dynamic walkthrough are proposed to be improved as shown in Alternative 2.

The aerial exhibit representing Alternative 3B can be viewed on the project website. Note that the shared use path is shown on the south side of Plainfield Road in red and the sidewalk is shown on the north side of Plainfield Road in blue. The existing ROW is shown as a red dotted line.

The three-lane typical section described in Alternative 2 would be provided up to the intersection of Brainard Ave.

The improvements at the intersection of Brainard Ave would, again, include northbound and southbound right turn lanes.

Through this segment, a 5-lane section would include a continuous right turn lane between Brainard Ave and 55th Street, to provide safer access to the various commercial driveways. A shared use path would continue on the south and the

sidewalk would continue on the north side of Plainfield Road.

As previously mentioned, the lane configuration at the intersection of 55th Street is not suggested to change.

Beyond 55th Street, Plainfield Road would then continue as a 3-lane typical section for the remainder of the corridor with a shared us path on the south and sidewalk on the north sides of Plainfield Road as described in Alternative 2.

Alternative 3B Pros and Cons

Pros	Cons
<ul style="list-style-type: none"> • Improves Level of Service throughout corridor • Provides consistent accommodations for pedestrians and bicycles • Provides opportunities for drainage improvements • Provide additional segment mobility for motorists • Improves safety by providing a refuge for left turning vehicles • Improves operational efficiency and safety for vehicles turning right into the various commercial driveways between Brainard Ave and 55th Street 	<ul style="list-style-type: none"> • Additional Right of Way impacts to provide additional lane • No significant operational improvements compared to impacts • Minor impacts to section 4(f) property at Arrowhead Memorial Point Park at the southeast corner of Wolf Road and Plainfield Road will be impacted by intersection improvements and shared use path • Shrubbery and tree removal required • Impacts to existing parking

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Alternative 3B and Alternative 3A have similar pros and cons. Alternative 3B again improves the level of service at intersections throughout the corridor, provides continuous accommodations for pedestrians and bicycles, provides opportunities for drainage improvements, and provides additional segment operational efficiency and safety. The continuous right turn lane in each direction provides additional capacity and safety for the numerous driveways for right turning vehicles between Brainard Avenue and 55th Street.

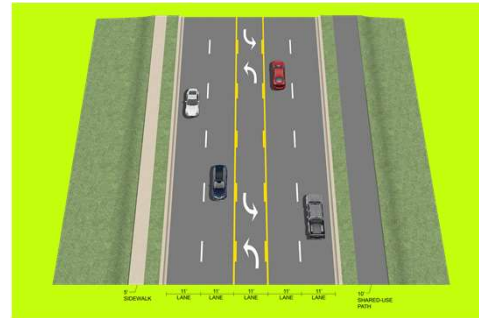
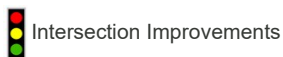
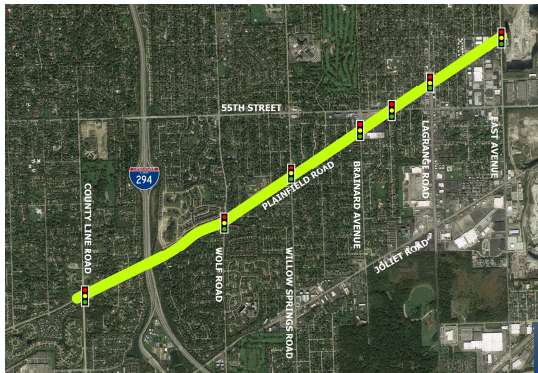
The additional lane is anticipated to result in additional ROW Impacts to provide the required roadway configuration. The limits of the roadway are anticipated to encroach on existing sidewalk and parking stalls on the southern side of Plainfield Road. As noted in Alternative 3A, an increase in impervious area would cause larger stormwater impacts that need to be addressed. Similar to the other alternatives, Alternative 3B would result in section 4(f) impacts to Arrowhead Memorial Park at the intersection of Wolf Road and Plainfield Road.

Alternative 3B is recommended for further investigation. Similarly, to Alternative 3A, IDOT design requirements and projected traffic data between Brainard Avenue and 55th Street supports the need to continue analyzing a 5-lane section in this area.



Lastly, we will look at Alternative 4 which consists of a continuous 5-lane roadway for the entire corridor. As with the previous alternatives, this alternative will also include all of the recommended intersection improvements from Alternative 1.

Alternative 4 Typical Section



- Intersection Improvements
- 5-lane section
- Continuous sidewalk
- Continuous shared use path
- Curb and gutter

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Alternative 4 would include a 5-lane roadway, shared use path, and sidewalk between County Line Road and East Avenue. Alternative 4 would also include intersection improvements as suggested in all the previous alternatives discussed.

The 5-lane roadway is not warranted through many segments of the corridor resulting in unnecessary impacts to adjacent ROW, stormwater, and environmental resources.

Because Alternative 4 is not being recommended for further, a dynamic walkthrough has not been prepared.

Alternative 4 Pros and Cons

Pros	Cons
<ul style="list-style-type: none">• Improves Level of Service throughout corridor• Provides consistent accommodations for pedestrians and bicycles• Provides opportunities for drainage improvements• Provides provide additional segment mobility for motorists• Improves safety by providing a refuge for left turning vehicles• Provides additional capacity in warranted segments	<ul style="list-style-type: none">• The 5-lane section is not warranted through many segments of the corridor resulting in unnecessary impacts to adjacent Right of Way and environmental resources• Arrowhead Memorial Point Park at the southeast corner of Wolf Road and Plainfield Road will be impacted by intersection improvements and shared use path• Additional environmental impacts resulting from additional through lanes• Additional property and ROW impacts resulting from additional through lanes• Municipal opposition to 5-lane section• Shrubbery and tree removal required• Additional storm water management required• Impacts to existing parking

Alternative 4 contains all of the benefits previously discussed, with additional traffic capacity and improved level of service throughout the corridor.

Although more capacity would be provided, it would well exceed the needed requirements for the majority of the corridor based on IDOT design criteria. The 5-lane roadway would result in additional anticipated impacts to adjacent properties and environmental resources that do not outweigh the operational benefits. Alternative 4 is not recommended for further investigation.

Alternative	Summary of Improvements	Recommendations
Alternative 1	<ul style="list-style-type: none"> • Intersection Improvements Only 	Not Recommended for further investigation
Alternative 2	<ul style="list-style-type: none"> • Intersection Improvements • 3-lane section • Continuous sidewalk/ Shared Use Path • Curb and gutter 	Recommended for further investigation
Alternative 3A	<ul style="list-style-type: none"> • Intersection Improvements • 5-lane section at select locations • 3-lane section at select locations • Continuous sidewalk/ Shared Use Path • Curb and gutter 	Recommended for further investigation
Alternative 3B	<ul style="list-style-type: none"> • Intersection Improvements • 5-lane section at select locations • 3-lane section at select locations • Continuous sidewalk/ Shared Use Path • Curb and gutter 	Recommended for further investigation
Alternative 4	<ul style="list-style-type: none"> • Intersection Improvements • 5-lane section • Continuous sidewalk/ Shared Use Path • Curb and gutter 	Not Recommended for further investigation

This table provides a summary of each of the developed alternatives and the Project Team’s recommendation based on the initial review of Alternatives.

Alternatives 1 and 4 are not recommended for further investigation, while Alternatives 2, 3A and 3B are recommended for further investigation.

As the alternatives are refined and the environmental clearances are obtained, the project team will evaluate impacts and prepare an impact analysis for the alternatives being considered that will help determine the preferred alternative.



Now we will wrap up the presentation with next steps.

Next Steps

- Public comment period ends May 5, 2021
- Project team to review public comments and feedback and begin alternatives evaluation
- Alternatives' evaluation to occur through 2022
- Second public meeting to take place in summer 2022 (Preferred Alternative)
- Project updates to be provided via e-newsletter and on project website:

ImprovePlainfieldRoad.org

**Please join us for the live Virtual Public Information Meeting at 6 p.m.
on April 14, 2021**

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The public comment period for this public information meeting ends May 5th, 2021

After the comment period closes, the Project Team will then review all public comments and feedback, evaluate the alternatives using the established evaluation criteria, and select a preferred alternative

The Project Team will then hold the second public meeting next year to present the preferred alternative

In the meantime, the Project Team will provide updates via an e-newsletter summarizing this meeting and comments we receive and will post ongoing project updates to the project website: [ImprovePlainfieldroad.org](https://www.improveplainfieldroad.org)

As noted on the slide, we are hosting a live virtual public meeting for the project on April 14th, 2021. Please visit the website for additional information and join us for that meeting.

Comment Period



Comments on the Public Information Meeting content will be accepted until May 5, 2021



Check out the project website: [ImprovePlainfieldRoad.org](https://www.improveplainfieldroad.org)



Email the project team: info@improveplainfieldroad.org



Leave a message on the project phone line: (708) 320-1485



Mail in your comment form:

**Plainfield Road Study c/o Metro Strategies Inc.
17 N. State, Suite 850 Chicago, IL 60602**

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Public comments will be accepted and added to the project record by May 5, 2021.

You can comment on the project by visiting the project website, emailing the Project Team at Info@ImprovePlainfieldRoad.org, leaving a message on the project phone line at (708) 320-1485, mailing your comment to Plainfield Road Study c/o Metro Strategies Inc. 17 N. State Street, Suite 850, Chicago IL 60602, or viewing hard copies of materials and filling out comment cards at Burr Ridge Village Hall, Countryside City Hall, Indian Head Park Administrative Offices, La Grange Village Hall, Western Springs Village Hall, and Township of Lyons office.

You can also join our live question and answer session which is part of the Public Meeting on April 14th.

Thank you

Contact the project team:

Sanjay Joshi, P.E., Project Manager

Info@ImprovePlainfieldRoad.org

Cook County Department of Transportation and Highways
69 W. Washington St., 24th Floor, Chicago, IL 60602

Thank you for joining us for the Plainfield Road Study public information meeting!